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The park lands at the Holy Cross College provide a huge swathe of pristine park land to the local community. This land is used by wildlife and also by locals for walking, relaxing, picnics etc. It is an oasis in central north Dublin. In this way it serves to soothe some of the stresses and strains of inner

city living in general and the even more pressured situation in Drumcondra outlined above in particular.

Reasons to be taken seriously (and Google)

High density housing whilst appearing “poor” provides a greater tax take per square km than other areas of Dublin. A high tax take area should see a corresponding tax funded investment (services, amenities etc.)

The Google factor – at some point Google (or Facebook or...) will move to a different city if their engineers cannot find reasonably priced accommodation with European standard amenities including parks and nature trails etc. and pollution alleviation – including parkland. Dublin is in danger of killing the goose.

Existing and future European laws concerning provision of amenities in cities and the alleviation of traffic noise and pollution etc. according to density of population...

Goals

The immediate goal of issuing this document at this time (August 2019) is to limit the extent of the destruction of the parklands at High Cross College. This includes in particular the preservation of the Tolka along its length on these lands – and the preservation of the old trees along the Drumcondra Road (acting as they do to alleviate some of the noise and exhaust pollution on the Drumcondra Road) and the preservation of public access.

This initial goal is essential for securing the overall goal, which is to link up the Tolka walkways from Tolka Valley Park to the sea.

Inspiration from Zurich, Switzerland

Switzerland is known the world over as a leader in integrated urban planning and design.

The steep walled Sihlgraben in central Zurich was long thought inaccessible for pedestrians; in fact it was historically the city’s defensive moat. This was until in the 90s when it was opened up for public use, achieved by the installation of walkways in stone and wood almost at river level.

Stretching over several kilometres, these routes are now extremely popular with locals and tourists alike and provide areas of peace in a busy business city as well as opportunities for businesses along the route.





These walkways allow the river to continue to flood as before. At these times the walkways are not usable.

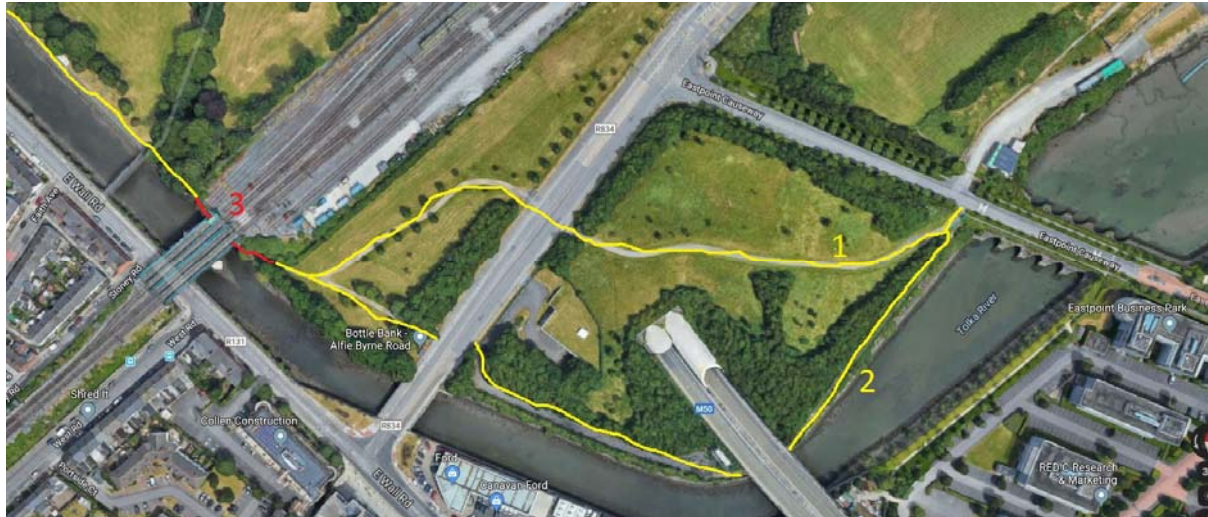
The Route

The route is shown in sections from east to west.

In the following sections:

- Yellow lines show “existing” paths. These are either existing paths or in the case of roads these are suggestions for routes which may initially be used as alternatives until the walkway is fully complete.
- Red lines show paths that need to be built.

East Point Business Park to the Annesley Bridge



The starting point by the East Point Business Park is a suggestion. The cycle path from the Clontarf “stops” near here. The path 1 already “exists”, path 2 seems to exist on satellite images – but has not been checked. The section 3 underneath the Dart/IR “Clontarf” bridge already exists but is fenced off(!) – the proposal is to open this connection:



(above) the fenced off path underneath the railway bridge at Clontarf – leading directly into Fairview Park.

Effort required

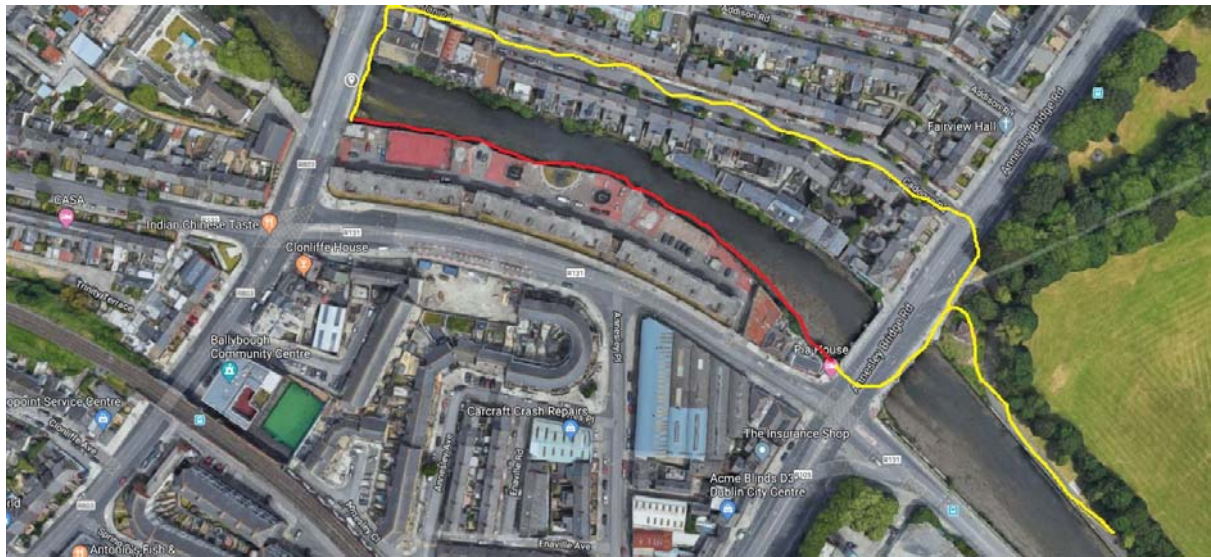
This section needs little in the way of new works

Pictures along the route (1):





Annesley Bridge to Luke Kelly Bridge

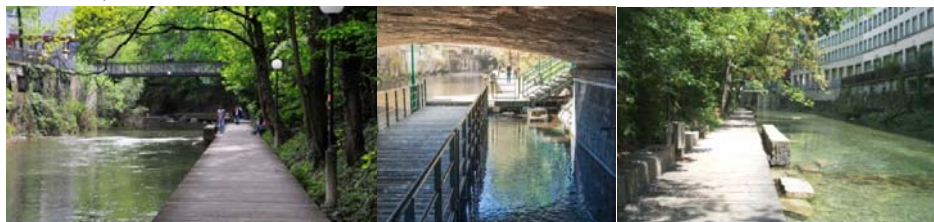


This is a particular sad, boxed-in part of the the river. It is pretty much a concrete canal.

Here the proposal is to continue along the Tolka on the south side of the river. Here there are 2 possibilities.

1. A board walk – high up like the Liffey board walk
2. A low level stone and wood walk just above river level (as used in Zurich Switzerland) Here when river floods the path is flooded too.
3. Using this “stone wood walk” system - at all bridges it is in theory possible to pass the path directly underneath the bridge
4. Along this section some “beautification” would be desirable – it is just very, grey and sad here.

Techniques used:



Or a boardwalk as on the Liffey

Effort required

This section needs major new works. To the north a “temporary road” solution is shown.

Pictures along the route:



(above) Luke Kelly bridge looking east – the walkway here would be on the right-hand side.



(above) Luke Kelly bridge looking west

Luke Kelly bridge to the Distillery



Here again a board walk or level “stone wood” path could continue along the south side.

Or, instead, the north side could be used. There is a green fenced off area at “1” which may be used – followed by an existing but perhaps private boardwalk (yellow dots) leading “almost” to the community centre.

Techniques used:



Or a boardwalk as on the Liffey

Effort required

This section needs major new works whether on the north or south side of the river. To the north a “temporary road” solution is shown – with access to the river again at the Clonliffe community center.

Pictures along the route:



(above) Looking east from the “Distillery” bridge – existing boardwalk visible



(above) Looking west from the bridge



This section needs some new works on the south side of the river. Here a low “river level” walkway or a boardwalk would be necessary – but only as far as the lands of the Holy Cross College (earlier if access can be agreed with the Belvedere School to a thin stripe of land along the river). A temporary alternative across the bridge and into the Belvedere grounds may be feasible.





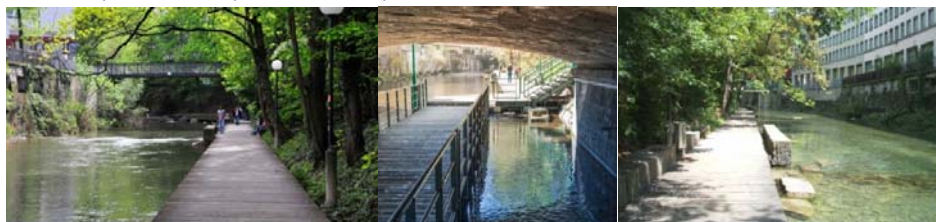
Holy Cross College to Botanic Avenue



The “red” route shows a foot bridge to Shelbourne FC – this links along the river to the west – but also opens up the Holy Cross College park to pedestrians from the north. A walkway along the river either north or south side would emerge underneath the Drumcondra Road bridge (here the southside would directly access the existing park).

The Yellow route essentially already exists – particularly the southern most route – here only the currently locked “emergency access” gate needs to be opened.

Techniques used (red section):



Or a boardwalk as on the Liffey

Effort required

This section needs some major works if the northern most route is taken remaining directly on the river with low level river walkways or boardwalks – this walkway may be on the north or south of the river.

As mentioned above the southern most walkway involves almost no effort – some pedestrian friendly traffic lights at the park exit would be desirable

Pictures along the route



(above) closed gate (on the right) to the lands of the Holy Cross college.



Botanic Avenue to Griffith Park



Techniques used (red section):



Or a boardwalk as on the Liffey

Effort required

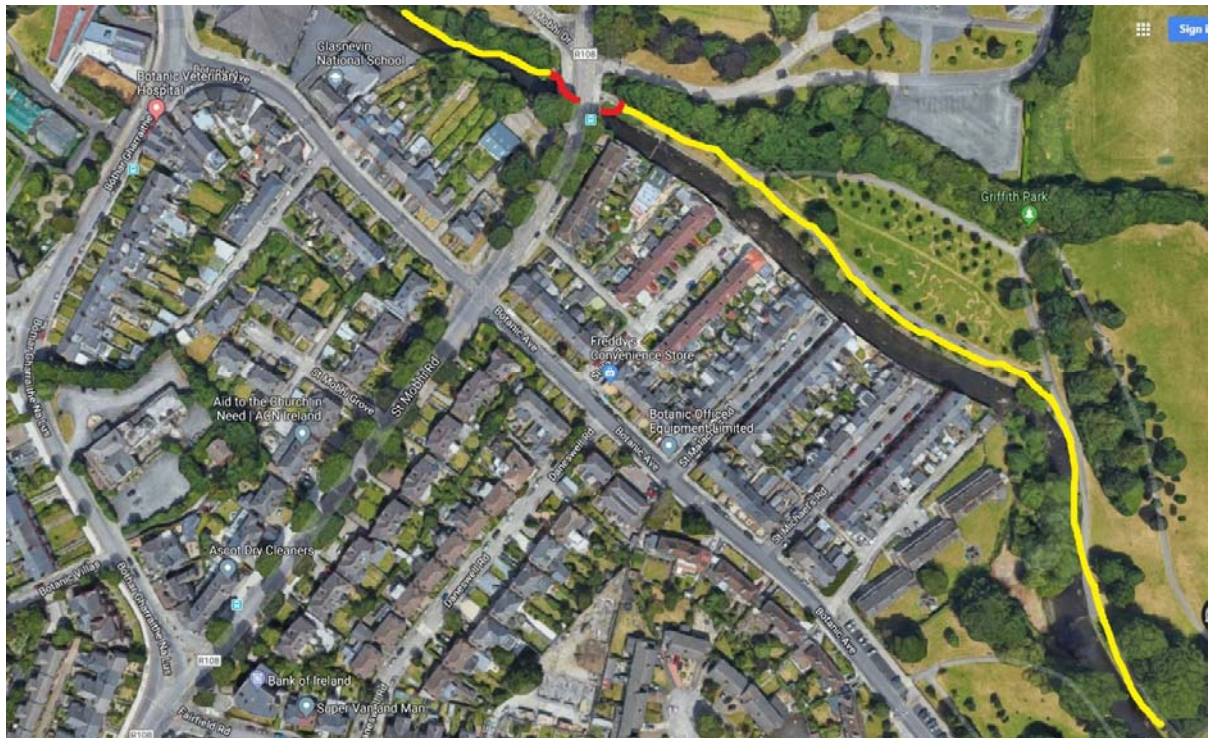
The yellow section already exists. The red option would require the building of a walkway. A continuous walk along the river would be the most desirable!

Pictures along the route





Griffith Park to St. Mobhi Drive



This yellow path exists to a large extent already. The only additional proposal here would be to build a footway underneath the bridge – shown in red. This would take pedestrians away from traffic. To the west this would connect directly to the existing river level walkway (currently closed) alongside St. Mobhi Drive.

Techniques used (red section):



Effort required

Reopening the existing river level walkway along the river at St. Mobhi Drive. More effort needed if a connecting under bridge walkway is installed.

Pictures along the route:



(above) Bridge St. Mobhi Road

St Mobhi Drive past the Tolka House through Convent/Botanics Riverside



Again the yellow route exists already – and would be easiest to “implement”. The only issue would be agreeing access to Convent lands to the north of the Tolka. A slim strip of this land directly by the river would suffice.

Techniques used (red section):



Or a boardwalk as on the Liffey

Effort required

The yellow section already exists. The red option would require the building of a walkway/boardwalk. Some integration with the Botanic's Rose Garden may be possible...

Pictures along the route:

The Convent part of the river, opposite the Botanics is simply stunning – almost like being in a different time. Great care would be necessary to preserve as much of this atmosphere as possible.





Convent/Botanics Riverside to Addison Park



Pictures along the route:

Again, the Convent part of the river, opposite the Botanics is simply stunning – almost like being in a different time. Great care would be necessary to preserve as much of this atmosphere as possible.



(above) Looking east



(above) Looking west along the Tolka





(above) The same path looking east





Addison Park to Tolka Valley Park



Effort required

Minimal effort needed on this section as the paths mostly already exist. A walkway underneath the bridge (Finglas Rd.) could be used to directly link the existing pathways and take pedestrians away from traffic.

Pictures along the route



